

Village of Midlothian Cicero Avenue Corridor

Final Plan

January 2011



Prepared by:
Hitchcock Design Group

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Executive Summary

Dear Community Leaders:

This Final Plan summarizes our three-phased study of the Cicero Avenue Corridor including the adjacent commercial areas along 147th Street. In addition to this Executive Summary, the report includes a brief Background of the project, the Goal and Objectives, and a summary of the previously completed Opportunity Analysis. All of this information forms the basis for the improvement recommendations outlined in the Corridor Plan section along with specific budget costs, priorities and actions described in the Implementation Strategy. Finally, schematic streetscape budget costs are provided in Appendix A and the overall streetscape schematic plans are provided in Appendix B.

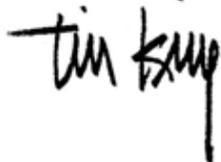
Although we continue to see tough economic times nationally, regionally and locally, we are encouraged that redevelopment will likely occur within the Cicero Avenue and 147th Street Corridors in the near future. The recent closing of adjacent auto dealerships along Cicero Avenue has made a relatively large parcel of property with great visibility available, providing an opportunity for significant redevelopment. Additionally, the Village has recently established a TIF district for the project area which will finance important public improvements. It is reasonable to expect that once a significant redevelopment occurs and public improvements are initiated, other private reinvestment will soon follow, reinforcing the area as a viable commercial destination.

Another important outcome of the Cicero Avenue Corridor study is the establishment of the Planning and Design Guidelines. Although redevelopment, public improvements and private reinvestment are all important factors, how the improvements are executed is critical to establish a level of quality and design that distinguishes the Village of Midlothian within the regional context. The Planning and Design Guidelines provide detailed information regarding the expectations that Village leaders can actively use as a tool to encourage the implementation of quality improvements.

Once the Village Board approves the Final Plan and Design Guidelines, a number of actions are recommended in the Implementation Strategy to proactively advance the plan. The Village is already one step ahead of the game with the establishment of the TIF district that will help to finance public improvements and support private reinvestment. Equally important will be for Village representatives to coordinate with property owners and developers to follow the recommendations outlined in the Planning and Design Guidelines. Also, advancing the planning and design of the public improvements such as gateway signage and streetscape improvements will demonstrate the public commitment to the project.

While challenging, we believe that it is possible for the Village to achieve its goal - to create an attractive and viable commercial destination that clearly distinguishes the community image of Midlothian within the regional context of the Cicero Avenue Corridor by 2015.

Sincerely,
Hitchcock Design Group



Timothy C. King, ASLA
Principal

Background

Introduction

Recently, a number of auto dealerships have closed leaving large areas of vacant property along the Cicero Avenue corridor in the Village of Midlothian. This fact combined with changing commercial building trends, has created an opportunity for the village to re-imagine what the Cicero Avenue corridor could be in the future. Rather than wait for re-development to occur naturally and hope that it will improve the quality of the community, village leaders have taken a pro-active position in an effort to direct the future of the corridor. The village recently created a Tax Increment Financing (TIF) District incorporating the project area, and the village has plans to update the Comprehensive Plan in the near future.

By adopting a well crafted Corridor Plan, the village will have an important tool that will not only establish a vision for the future of the corridor, but will help guide public policy and resource allocation, set the stage for the public improvements, and help facilitate negotiations with private land owners and developers.

Study Area and Scope of Work

The study area includes Cicero Avenue throughout the Village limits from 143rd Place on the north to just past 148th Street on the south, and 147th Street from Lamson Avenue on the west, to Knox Avenue on the east. The scope of work includes developing recommendations for public improvements throughout the project area, along with private site and building design guidelines for properties that front the two roadway corridors.

Process

In September of 2010, the Village of Midlothian retained Hitchcock Design Group (HDG) to complete a three-phased study. During the Opportunity Analysis, the Hitchcock Design Group team analyzed information about the study area to gain an understanding of the existing conditions and potential improvement opportunities. Next, the team interviewed approximately 30 selected project stakeholders to hear first hand opinions about the study area. Following completion of the Opportunity Analysis, the team developed alternative improvement strategies which were reviewed with the Plan Commission and presented at a public open house. Working with the Plan Commission and Village Board, the team then identified a preferred strategy and further refined the improvement recommendations. The Final Corridor Plan and Planning and Design Guidelines summarize the results of this process and the improvement recommendations.



Goal and Objectives

The goal for the Cicero Avenue Corridor Plan is intended to be an ambitious, visionary statement that will guide the development of improvement recommendations. The goal is based on an analysis of the current conditions and discussions with community leaders and project stakeholders.

Create an attractive and viable commercial destination that clearly distinguishes the community image of Midlothian within the regional context of the Cicero Avenue Corridor by 2015.

Each of the following objectives support the goal and will help Village leaders accomplish the desired outcomes.

Objective 1: Create a functional, practical design solution that can be implemented in phases

Improvements are likely to involve a number of public and private entities and will occur over a period of time. The plan must be flexible to accommodate this reality.

Objective 2: Develop a positive image, establish a unique sense of place, and improve the competitiveness and quality of life in Midlothian

There is a tremendous sense of community spirit and pride within the Village of Midlothian. This should be apparent when you enter the community and continue throughout the Cicero Avenue Corridor. Recognizing this public investment, private property owners and developers will want to reinvest in their properties.

Objective 3: Create site and building development guidelines that can be integrated into the Village Comprehensive Plan and Zoning Code requirements

The Village has direct control over what happens in the public right-of-way, but private property developments also have a significant influence on the quality and character of the corridor. It is important to establish reasonable site and building development guidelines in order to achieve the project goal.

Opportunity Analysis Summary

Land Use

The Cicero Avenue and 147th Street corridors within the study area consist primarily of commercial properties of varying sizes. Multiple auto dealerships used to dominate the Cicero Avenue Corridor and are now mostly vacant. The exception is the Ford dealership that is still operating and stable. There are a number of other automotive focused businesses, along with a variety of fast food restaurants and in-line retail centers. Mid-size shopping centers that were once state of the art are now struggling to compete with more contemporary big-box developments in nearby communities. However, recent private reinvestments include Walgreens, Krispy Kreme, White Castle, and a new hotel currently under construction. The vacant auto dealership properties consist of significant acreage and offer an attractive opportunity for redevelopment. It is reasonable to expect that if a significant new development were to occur on these properties, reinvestment would soon follow elsewhere within the study area.

Transportation and Access

The project area is dominated by automotive traffic with high traffic counts on Cicero Avenue (35,700 ADT) and 147th Street (18,500 ADT). High traffic counts are good for retail exposure but can be challenging for pedestrians and non-automotive uses. Parking and access to private properties is for the most part adequate, although multiple curb-cuts can make streetscape improvements challenging. Cross-access between individual properties could benefit businesses and traffic flows.

Pace bus routes run along Cicero Avenue and 147th Street, although ridership is low. Pedestrian activity is limited along the corridor and sidewalks are inconsistent and intermittent. Pedestrian crossings on Cicero Avenue and 147th Street are limited. There are pedestrian push buttons at the intersection of Cicero and 147th but no countdown signals. A new traffic signal has been discussed at the intersection of 145th Street and Cicero Avenue which would provide better access for future new development and a convenient crossing point for a proposed bike route along 145th Street.

Public Infrastructure

Although a detailed analysis of the public infrastructure was not part of this study, public utilities appear to be adequate for the existing and anticipated uses within the study area. There are overhead utility lines in portions of the study area although the majority of utilities are underground. Existing street lights along Cicero Avenue are village controlled and maintained. A new water main is planned for the west side of Cicero Avenue and it is likely that other public utility upgrades may be required over time to meet the needs of contemporary commercial redevelopments.

Regulatory Requirements

Although the Village of Midlothian is taking the initiative to improve the Cicero Avenue and 147th Street corridors, the village does not have direct control of the property within the project area. Public right-of-way on both roadway corridors is controlled by the Illinois Department of Transportation (IDOT) and the adjacent properties are all privately held. The Village does however have influence on physical improvements through building and zoning regulations. The majority of existing properties within the study area were developed before the current building and zoning codes were in effect. If private properties are improved or redeveloped, enforcement of the building and zoning codes will greatly improve the conditions within the study area.

It is important to note that any improvements within the public right-of-way will be subject to IDOT regulations. Also, new developments must comply with the Metropolitan Water Reclamation District of Greater Chicago (MWRDGC) stormwater management requirements.



Condition and Appearance

The overall condition and appearance of the study area is typical of many older suburban auto-oriented commercial corridors and can best be described as utilitarian. Although there is a strong sense of community spirit and pride within the village, the Cicero Avenue corridor does not reflect the unique culture of Midlothian.

Promising Opportunities

Based on the research and analysis conducted as part of the corridor study, the following items have been identified as the most promising opportunities for improvements to meet the goals and objectives.

Construct public improvements that reflect the community image

Public improvements such as streetscape, gateways, signage and amenities should be designed to create a unique sense of place based on the Village of Midlothian Scottish theme

Create a “Complete Street” streetscape environment

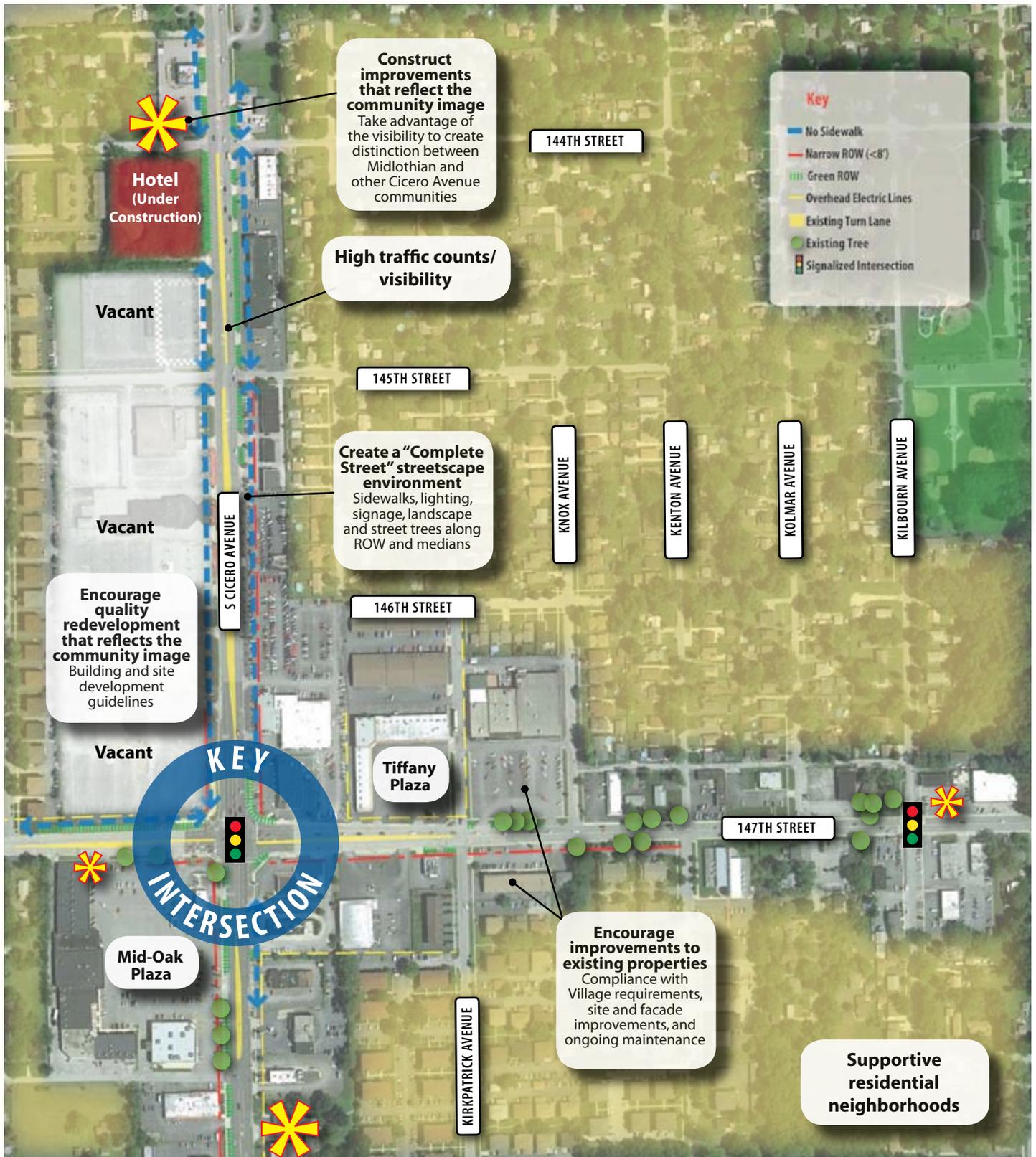
Although the study area is dominated by vehicular traffic and will likely remain a drive-to destination, it is important to provide improvements to accommodate all types of users. Items such as continuous sidewalks, accommodating transit stops, clear roadway crossings, consistent lighting, and attractive landscaping will create a holistic and safe environment

Encourage quality private redevelopment that reflects the community image

As properties redevelop, encourage building and site improvements that reflect the community image through the use of planning and design guidelines

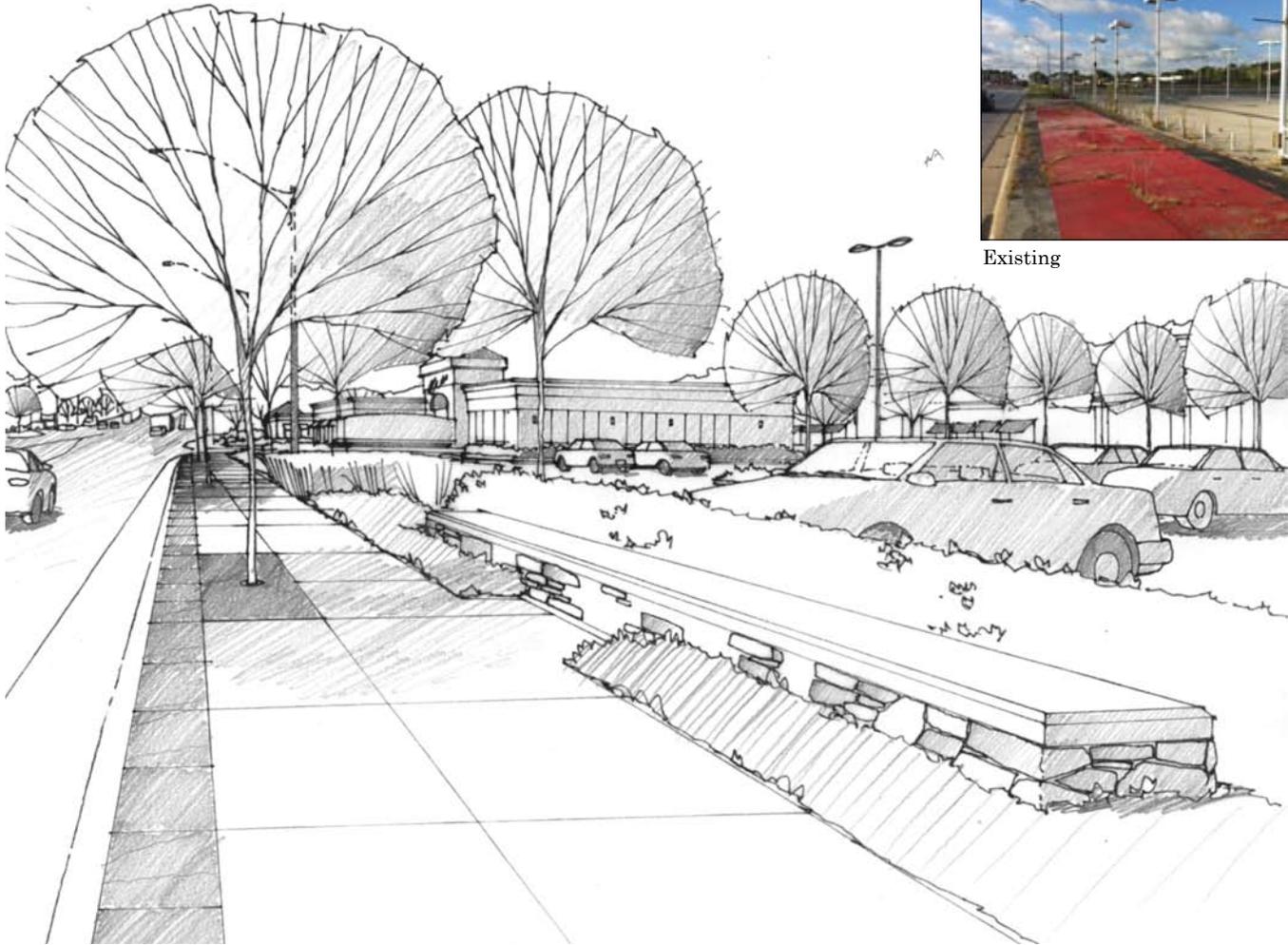
Encourage improvements to existing private properties

It is likely that a number of properties will remain and not redevelop for some time. Improvements to existing properties including compliance with current zoning requirements, façade improvements, and ongoing maintenance should be encouraged to improve the overall condition of the study area



Corridor Plan

The Corridor Plan includes public and private improvement recommendations that once implemented, will ultimately achieve the goal to “Create an attractive and viable commercial destination that clearly distinguishes the community image of Midlothian within the regional context of the Cicero Avenue Corridor by 2015”. The recommendations include streetscape improvements, gateway and identity elements, and planning and design guidelines for new developments and improvements to existing properties.



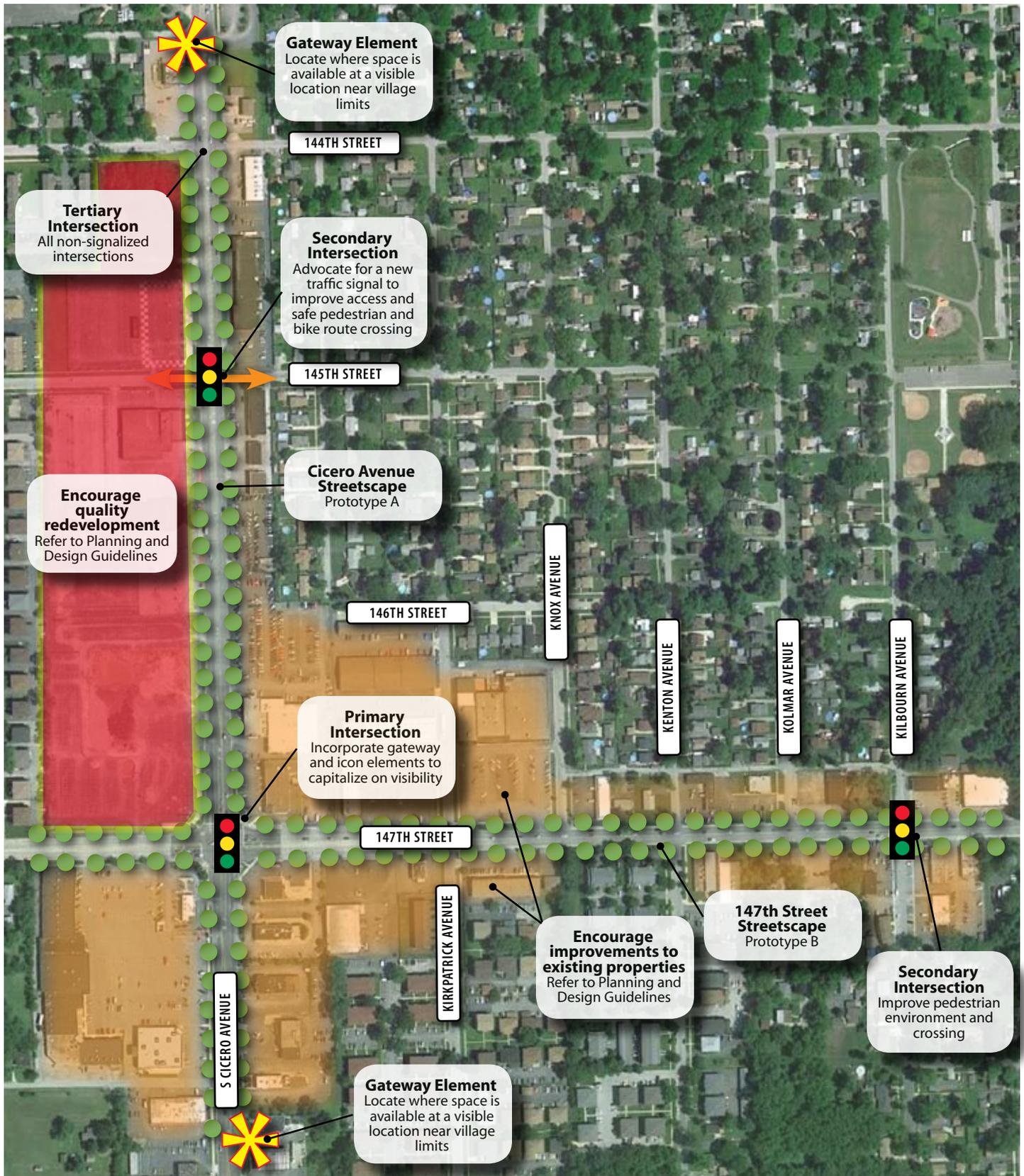
Existing

Cicero Avenue Streetscape

Streetscape Improvements

Streetscape generally includes all of non-vehicular items within the public right-of-way. Elements such as sidewalks, crosswalks, transit stops, furnishings, lighting, street trees, landscape and signage make up the streetscape environment and can benefit both public users and private property owners by providing safe, attractive and consistent access to businesses. When applied consistently, streetscape improvements can have a significant impact on the image of a community and create a unique sense of place.

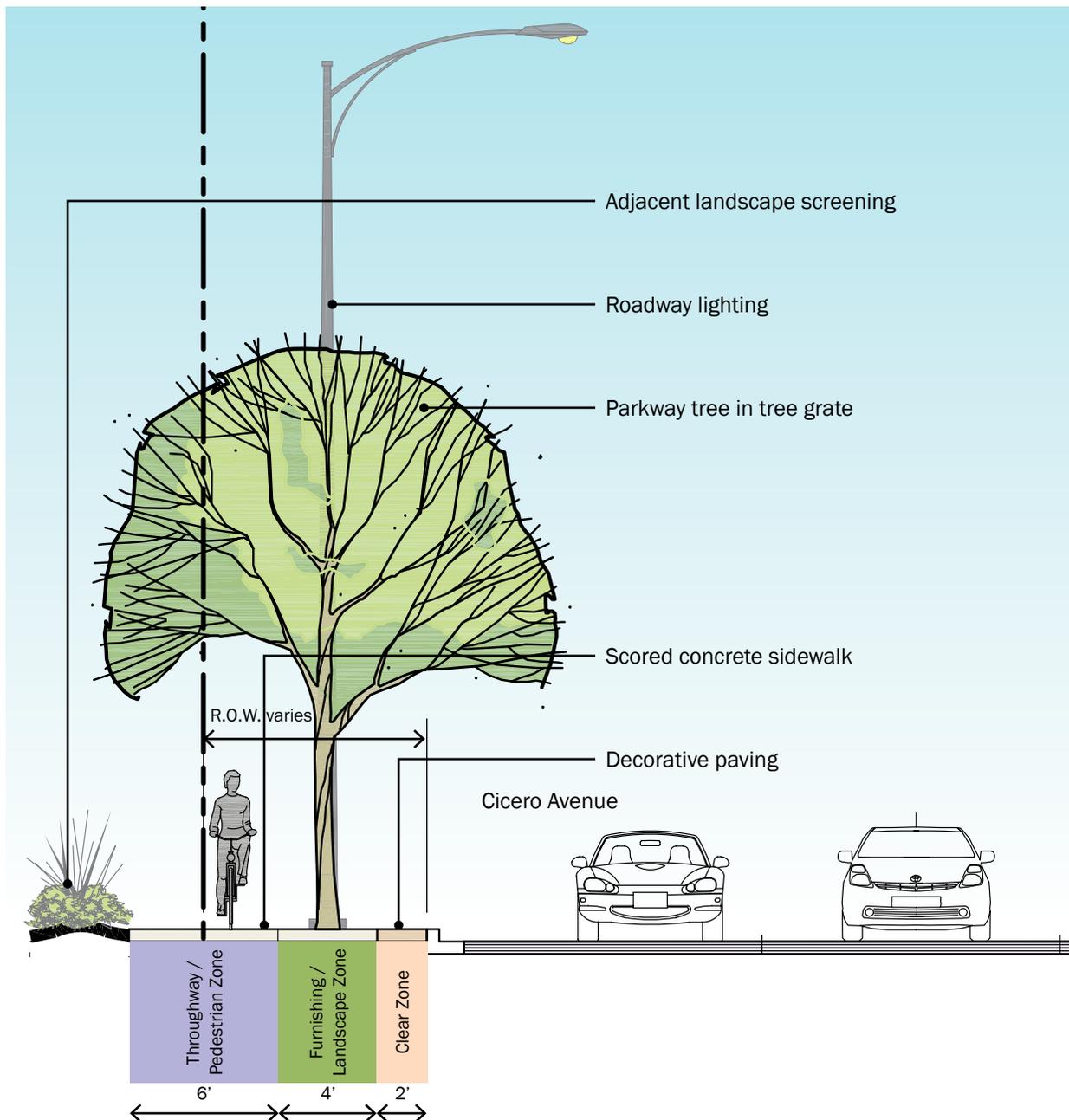
A streetscape consists of three distinct zones; the throughway/pedestrian zone, the furnishings/landscape zone, and the clear zone. It is often preferable to place the furnishings/landscape zone (sometimes called the buffer zone) between the throughway/pedestrian zone and the clear zone to provide a buffer between the roadway and the pedestrian environment. However, existing conditions such as the width of the public right-of-way, adjacent properties, and type of roadway often dictate what is realistically possible. For Cicero Avenue and 147th Street the following streetscape prototypes were determined based on a preliminary analysis of the existing conditions.



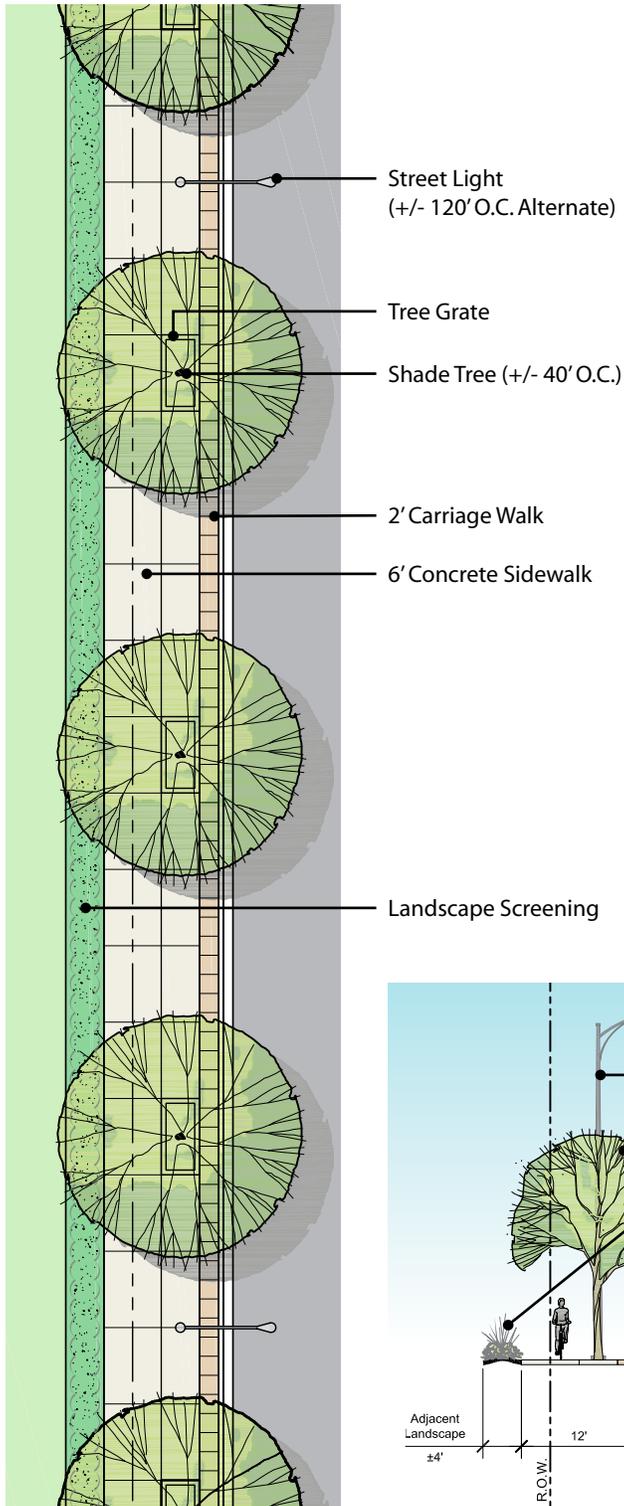
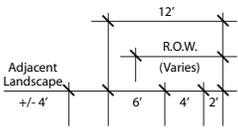
Streetscape Prototype A – Cicero Avenue Corridor

Arranging the streetscape with the furnishings/landscape zone between the clear zone and throughway/pedestrian zone is highly desirable along Cicero Avenue for two reasons: First, to provide a buffer between pedestrians and the fast moving vehicular traffic; and second, to provide a more significant, visible, and consistent streetscape that will improve the community image. The minimum width for this type of arrangement is 12 feet and it appears that this can be accomplished throughout the Cicero Avenue corridor without requiring significant modifications to conditions on adjacent properties.

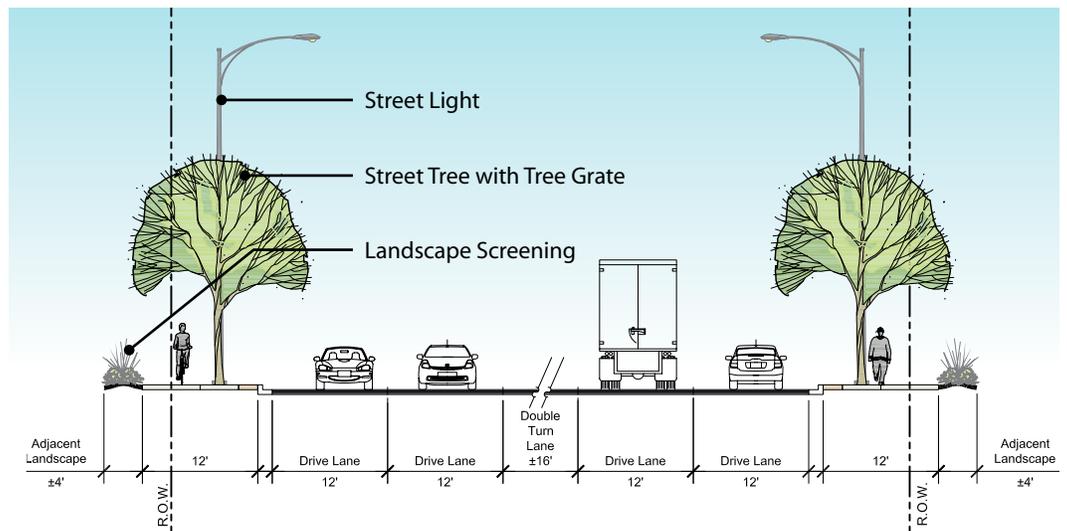
It should be noted that the right-of-way along Cicero Avenue is inconsistent from property to property. In many cases, streetscape prototype A can be applied easily within the existing public right-of-way. But in some situations it will be necessary to gain access to property outside of the existing right-of-way either through easements or other agreements with private property owners to consistently apply prototype A.



Prototype A - Use Zones



Prototype A - Comparable Image

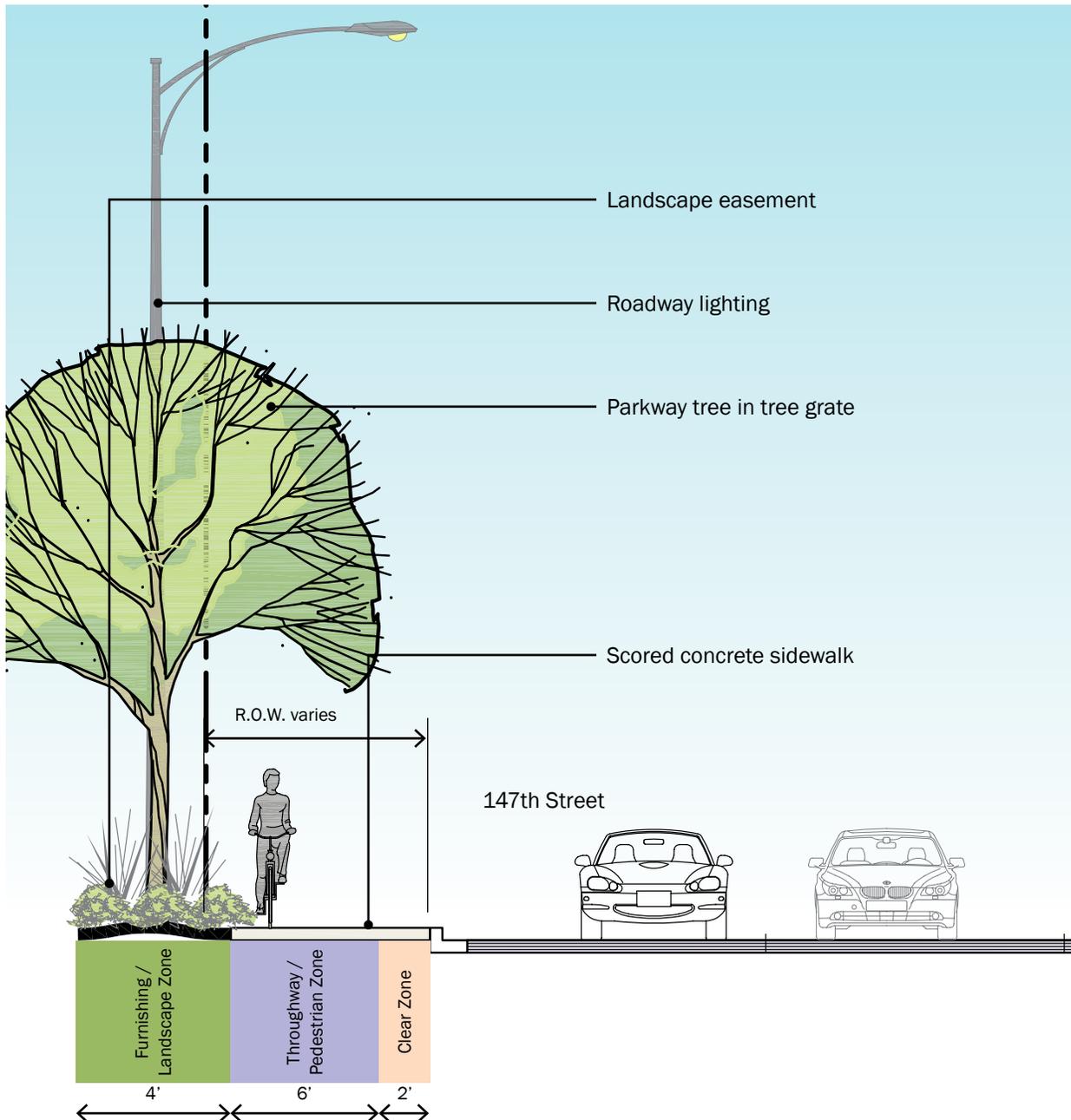


Prototype A - Plan

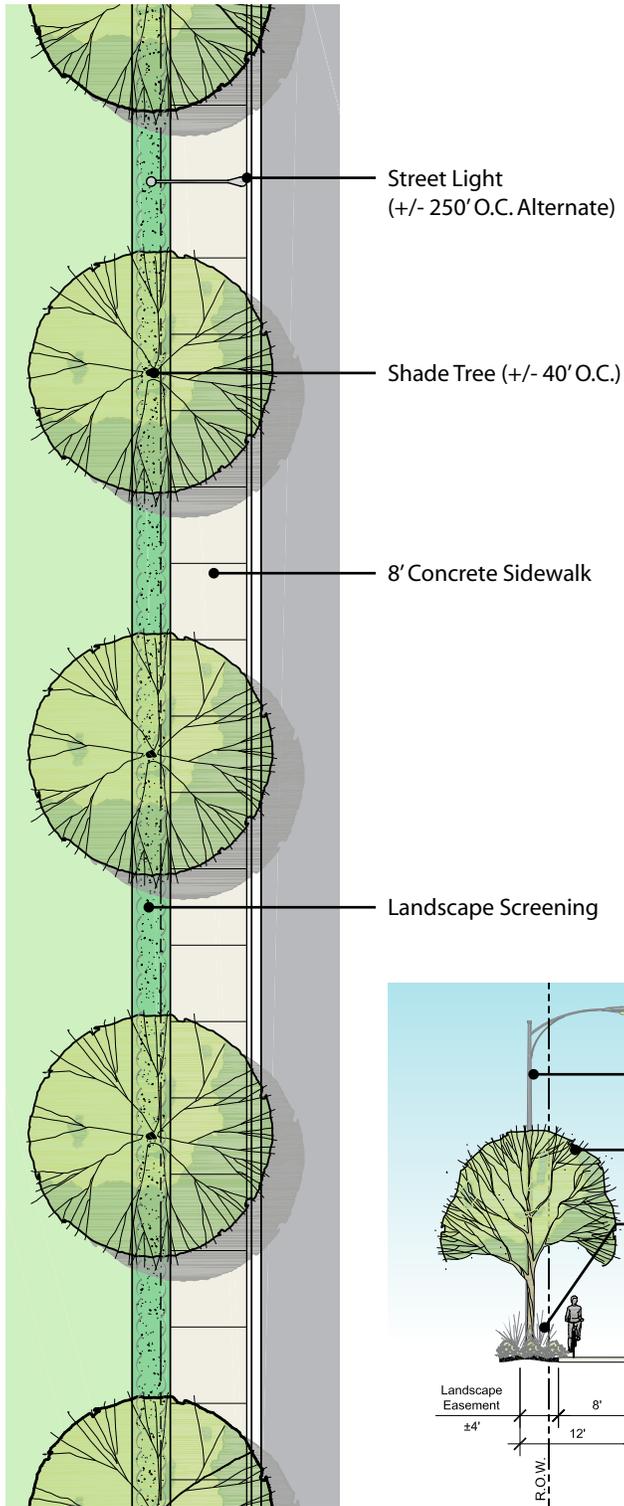
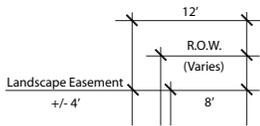
Prototype A - Section

Streetscape Prototype B – 147th Street Corridor

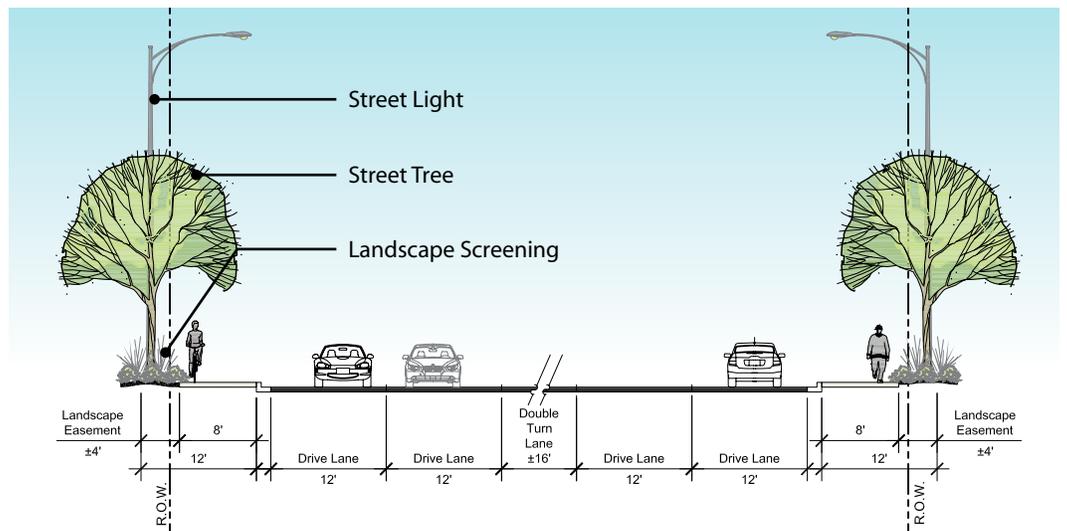
Along 147th Street the throughway/pedestrian zone is located directly adjacent to the clear zone. The furnishings/landscape zone is then located between the throughway/pedestrian zone and adjacent properties. This is due mainly to the fact that there is limited space and implementing prototype A would require significant modifications to adjacent properties. Since it is anticipated that many of the properties along 147th Street will not redevelop in the near future, it is more realistic to establish a streetscape prototype that requires less space and provides for more flexibility. The minimum width for this type of arrangement is 8 feet, with the furnishings/landscape zone being provided adjacent to private properties either as private improvements or in landscape easements where necessary. Although this might not be the ideal arrangement, it is acceptable because 147th Street has less vehicular traffic and visibility.



Prototype B - Use Zones



Prototype B - Comparable Image



Prototype B - Plan

Prototype B - Section

Primary Intersection – Cicero Avenue and 147th Street

The intersection of Cicero Avenue and 147th Street is a major intersection with high traffic counts and high visibility. This creates an opportunity to provide meaningful streetscape improvements but also poses significant challenges to pedestrian circulation and safety. The intersection is currently dominated by vehicular uses and there is very little right-of-way to allow for streetscape improvements. But with a bit of creativity and cooperation among the adjacent property owners, this intersection could be greatly improved to provide a safe pedestrian environment and unique sense of place as illustrated in the intersection enlargement plan.



Existing

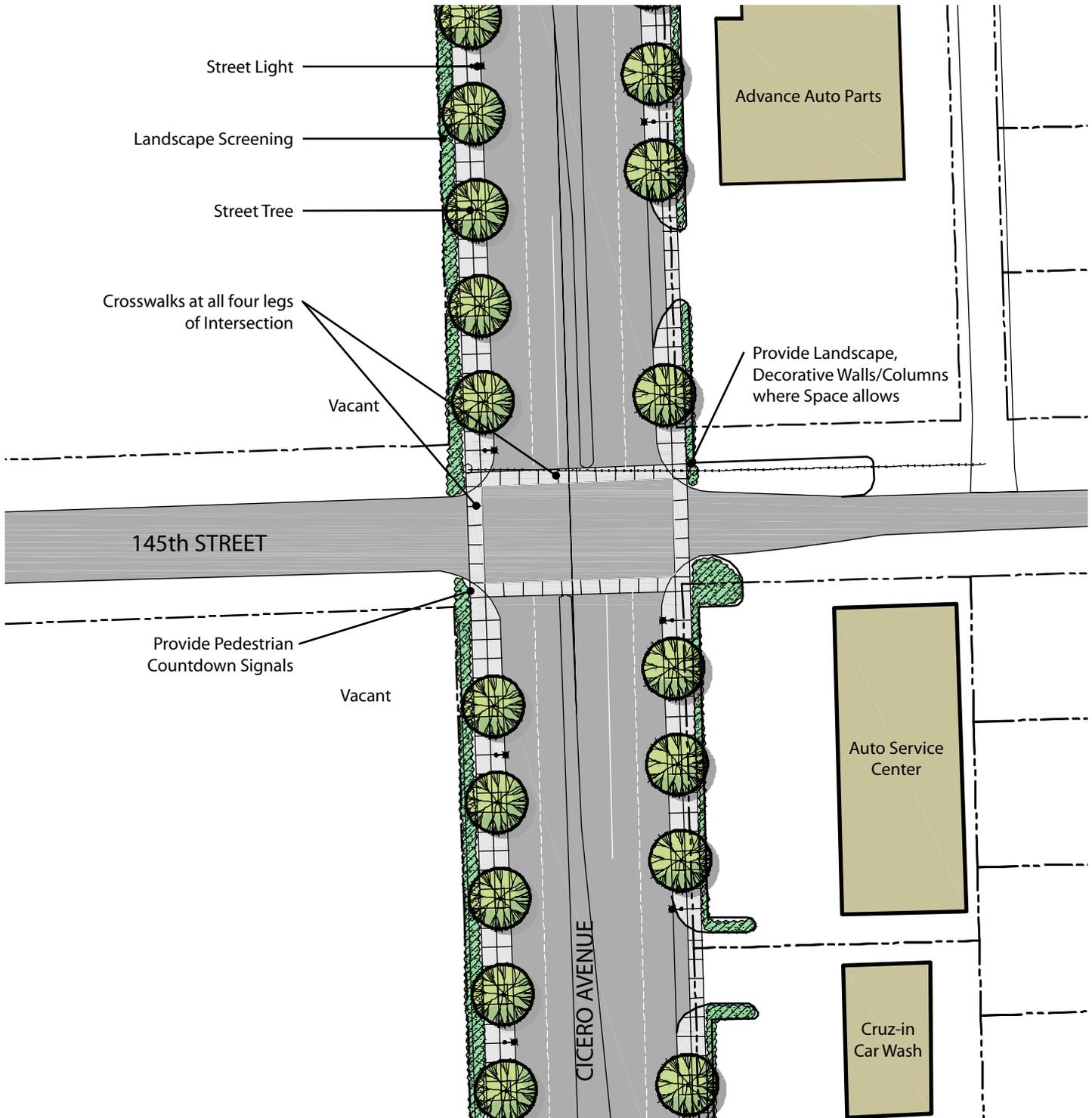
Cicero Avenue, 147th Street Intersection



Primary Intersection - Plan

Secondary Intersections

Improvements at signalized cross streets will also provide a safe pedestrian environment and contribute to the sense of place along each corridor. The intersection of 147th and Kilbourn is signalized, and it is anticipated that the intersection of Cicero Avenue and 145th Street will be signalized in the future. Each intersection is located at a good location to provide cross access for pedestrians and bicyclists to circulate throughout the community.



Tertiary Intersections

Consistent improvements at non-signalized cross streets will go a long way to improve the pedestrian environment along each of the corridors. This is also true for driveway crossings, where pedestrians should be given the right-of-way.



Tertiary Intersection - Plan

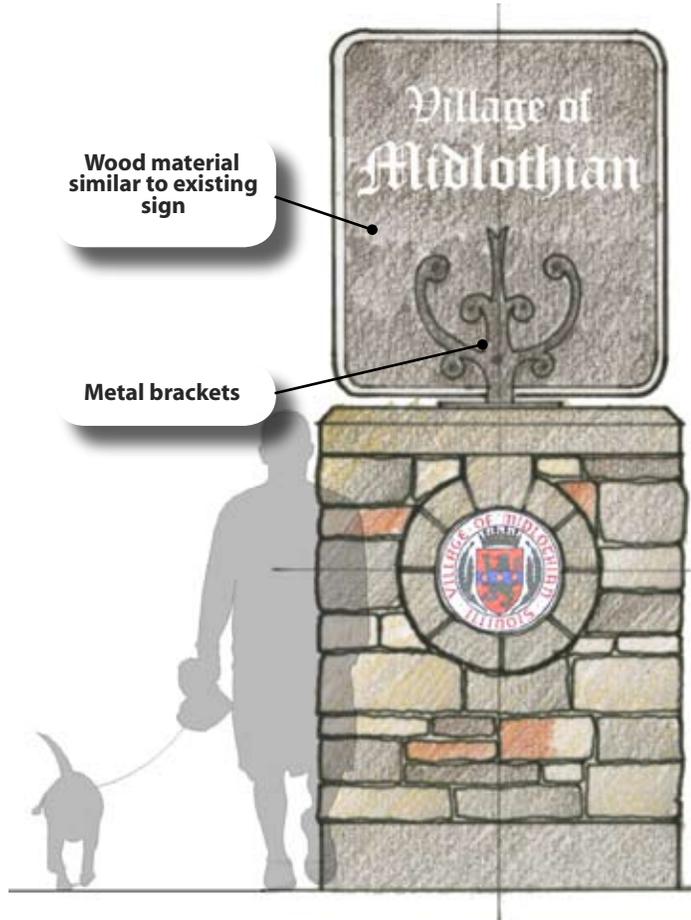
These prototypes will guide the detailed design of streetscape improvements along each corridor and will likely need to be adapted to reflect specific site conditions. Refer to Appendix A for a preliminary plan representing how the prototypes might be consistently applied throughout the study area creating a cohesive streetscape environment.

Refer to the “Cicero Avenue Corridor Planning and Design Guidelines” for specific streetscape material recommendations.

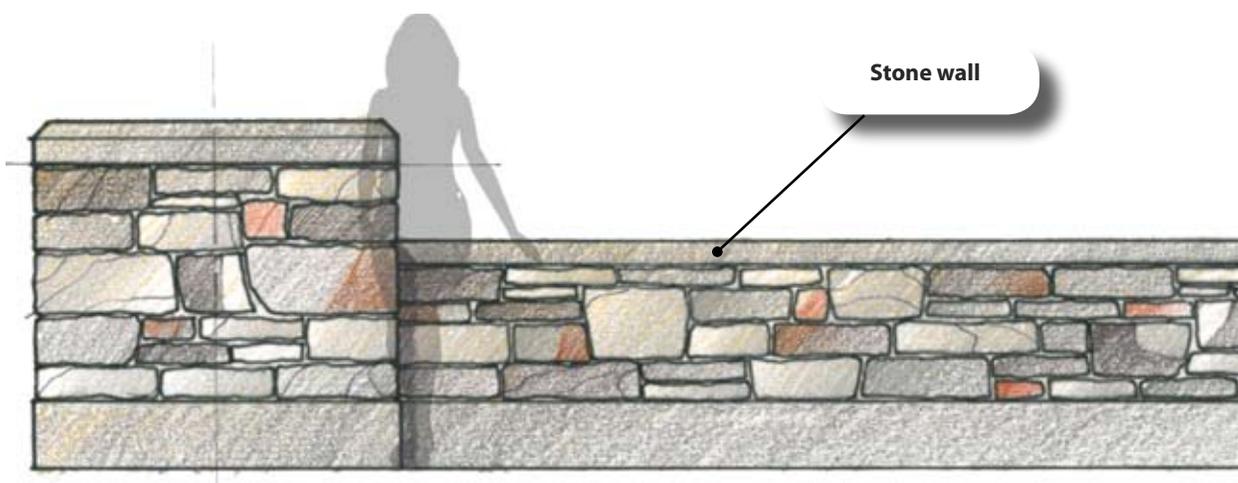
Gateways and Identity

Project stakeholders have clearly stated that the Midlothian Scottish theme is appropriate and should be developed further to reinforce the community image and establish a sense of place. Significant gateway elements, placed at the Village limits along the Cicero Avenue corridor will establish a sense of arrival and the continuation of signage elements with the Village name and crest throughout the corridor will provide consistency. Decorative walls and columns located at the Cicero Avenue and 147th Street intersection will also reinforce the community theme.

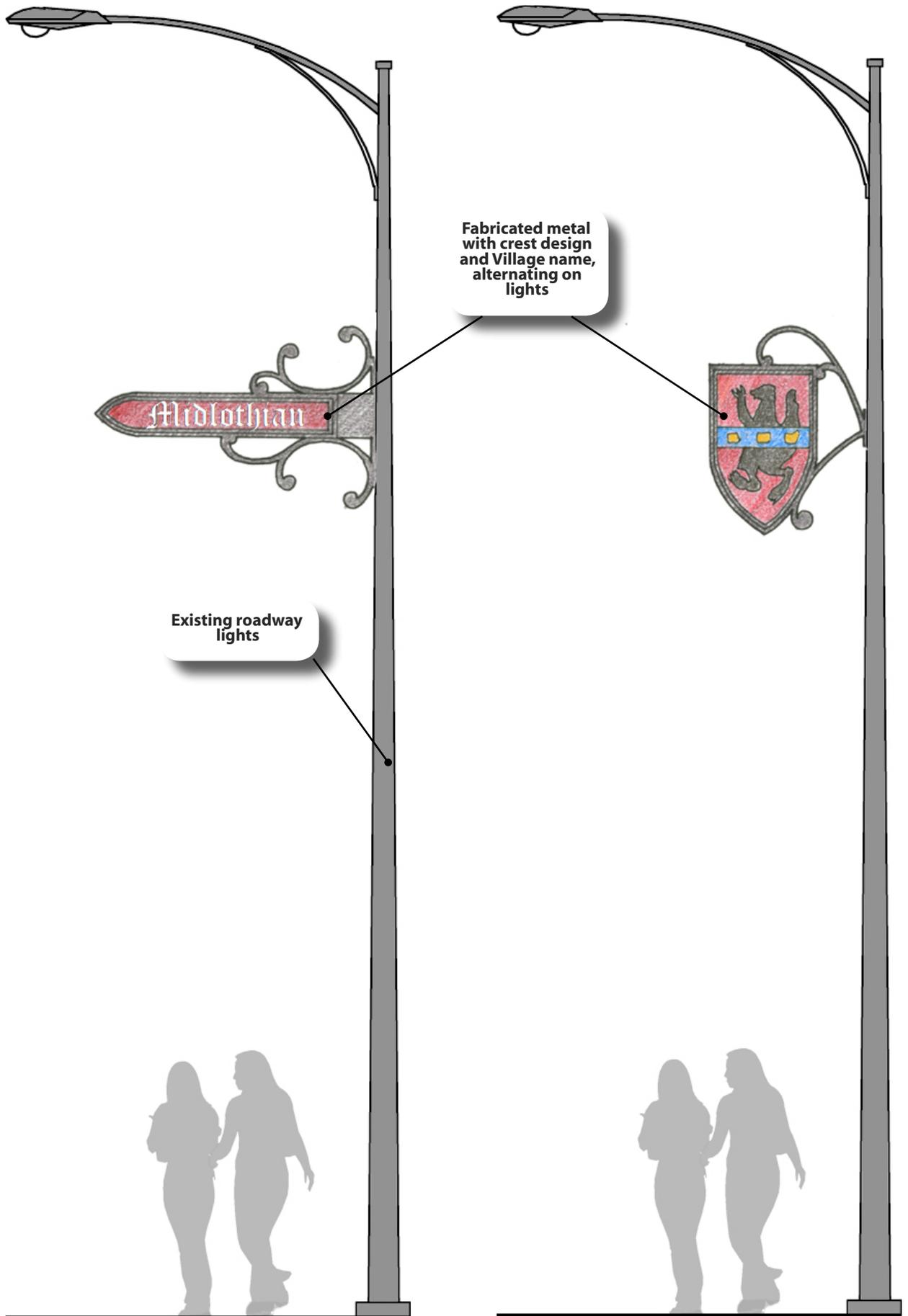
Improvements to private properties consistent with the community identity is encouraged and outlined in detail in the “Cicero Avenue Corridor Planning and Design Guidelines”.



Gateway Element



Decorative Wall and Columns



Fabricated metal with crest design and Village name, alternating on lights

Existing roadway lights

Icon Signage Elements

Planted Median Option

Installing planted medians is a common enhancement recommendation for major roadway corridors. Along Cicero Avenue and 147th Street there are a number of locations where planted medians could be installed without modifying the existing traffic lanes. A clear benefit of planted medians is to soften and beautify the harsh roadway environment, but a negative aspect is that the medians would also limit access to and from a number of properties. Planted medians are common where there are few driveway access points, typical of more recently developed corridors. As properties redevelop over time and driveway access is consolidated, it may be advisable to revisit the idea of adding planted medians.

Improvements to Existing Properties

In many circumstances, relatively simple improvements to existing properties to meet compliance of current building and zoning code regulations would greatly improve the conditions along each of the corridors. Items such as buffer yard landscaping, parking lot landscaping, signage and screening could be accomplished without many challenges.

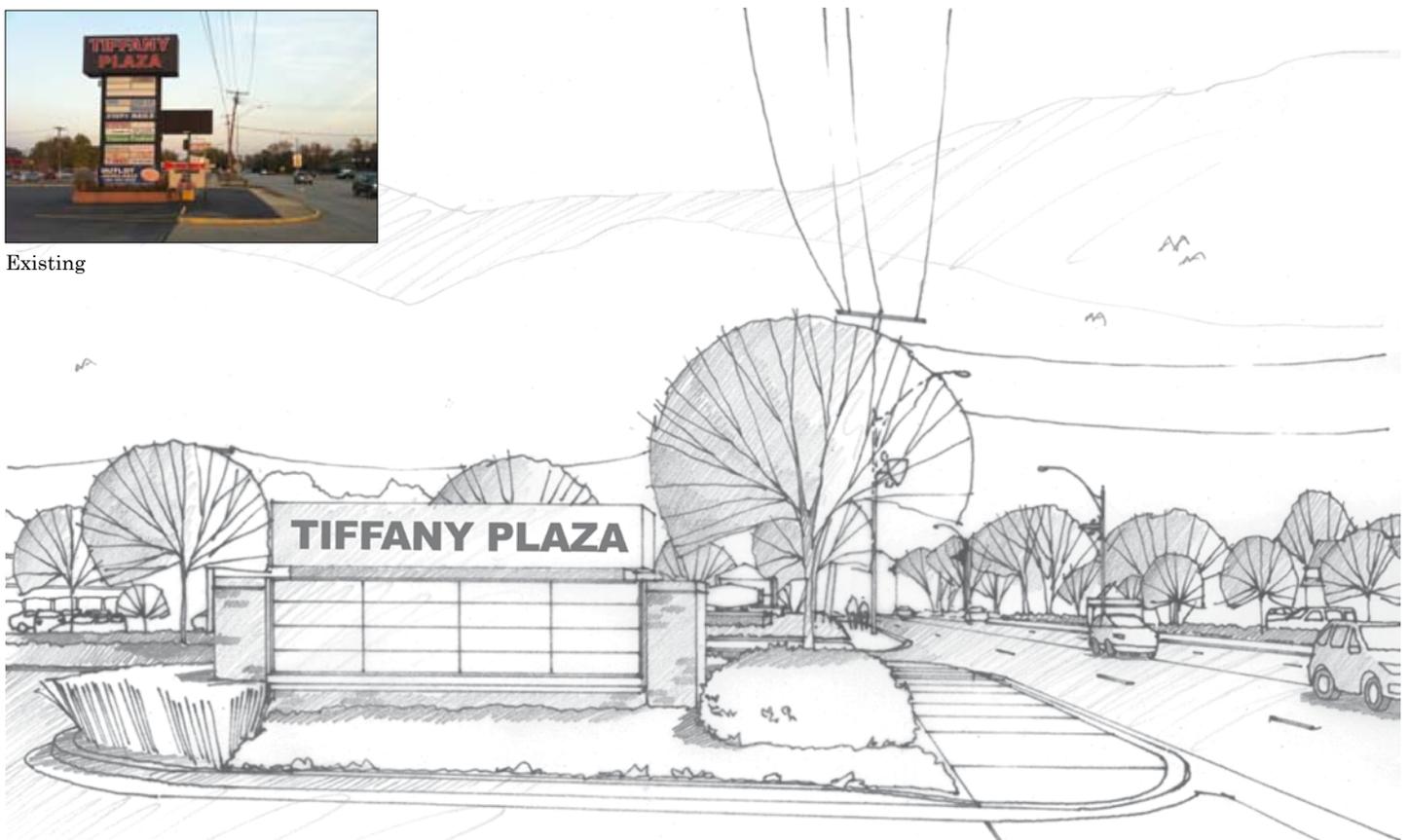
Additional items applicable to existing properties include circulation improvements, architectural treatments, lighting, furnishings, and plantings are outlined in the Corridor Planning and Design Guidelines.

New Development

New private developments offer a significant opportunity to improve the overall condition within the study area. New developments will not only be required to meet current building and zoning code regulations, but will also benefit from the recommendations outlined in the Corridor Planning and Design Guidelines. Applied properly, the regulations and guidelines can provide a positive outcome for the private developer and the village by creating a unique environment consistent with the community image.



Existing



147th Street Streetscape

Implementation Strategy

Although implementation of all of the recommended improvements at once would be ideal, it is more likely that improvements will need to be implemented in manageable phases over time. Numerous circumstances will affect the implementation priorities and should be evaluated continuously. The following short term strategies are outlined based on the current circumstances.

Gateway Signage

Gateway signage would have a significant initial impact and would demonstrate the Village's commitment to public improvements throughout the study area. Gateway signs can be constructed as stand alone individual projects which makes them ideal as initial short term initiatives.

Anticipated budget for each gateway sign: \$25,000

Streetscape Improvements

The schematic streetscape plans for Cicero Avenue and 147th Street included in Appendix B demonstrate how each corridor could be improved by applying the appropriate prototype streetscape design. Based on these schematic plans the cost to complete the Cicero Avenue Streetscape would be approximately \$3,000,000 and for the cost to complete the 147th Street Streetscape would be approximately \$1,600,000. It should be noted that these costs are preliminary based on schematic level plans and there are many variables that will affect the actual costs of construction. However, the costs outlined in Appendix B provide a starting point for planning, prioritizing, and budgeting purposes.

Improvements Associated with new Development

There are significant vacant properties that are likely to develop in the near future. Coordination with the developer to ensure that the Corridor Planning and Design Guidelines are utilized and the streetscape improvements are installed consistent with the recommendations should be a main priority. Negotiations regarding development costs, cost sharing, incentives, and other financial considerations will likely occur as part of the entitlement process.

It is anticipated that the streetscape improvements associated with the vacant properties would be completed in coordination with any new development. Based on the schematic streetscape plans, the streetscape costs adjacent to the existing vacant properties would be approximately \$750,000.

More detailed discussions between village officials and developer representatives will be necessary to determine specific development requirements and associated responsibilities and costs.

Existing Property Improvement Incentives

Requiring existing property owners to meet current regulations when they apply for modifications to existing uses, or proactively identifying unacceptable conditions and working with the property owners to meet an equitable solution can go a long way to improve the conditions in the study area. Other incentives such as establishing site and façade matching grant programs can also produce significant results.

Establish Funding

Many of the improvements outlined in this Corridor Plan are eligible for TIF funding. A TIF is currently being established for the study area and once in place, potential funding of the corridor recommendations can be outlined in detail. Additionally, the

corridor plan can be used to seek outside funding from programs such as the Illinois Transportation Enhancement Program (ITEP) which is particularly well suited for these types of improvements.

Special Considerations

The Corridor Plan and Planning and Design Guidelines are intended to establish a vision for the study area and act as working documents that will be referred often as opportunities arise. However, in order to take advantage of the most promising opportunities, particular attention will need to be paid to certain items that could be a challenge to implementation. Village representatives should proactively work to gain a clear understanding of these issues and to establish a more detailed work plan as more information becomes available related to property re-developments. Items that could prove challenging to implementation are outlined below.

Physical Space:

In certain areas along the corridors, there is limited space to construct new improvements. It will be necessary to negotiate with private property owners to allow for the desired improvements or to establish easements as needed.

IDOT Control:

Constructing unique improvements within IDOT right-of-way can be challenging. The village must be willing to negotiate with IDOT to obtain the required permits and be patient as reviews and approvals are being processed. IDOT will require the village to enter into an intergovernmental agreement for maintenance of any village initiated improvements. The village should initiate discussions with IDOT regarding the Corridor Plan to identify any potential issues that could affect new improvements.

Maintenance and Sustainability:

New improvements to meet the goals and objectives will require a higher level of maintenance than currently is required. The village will need to establish a maintenance plan, either publicly or privately, to ensure that the improvements are sustained over time.

Existing Property Participation:

Participation by existing property owners will be critical to the overall improvement of the project area. The Village may need to initiate conversations, offer incentives, or identify other types of encouragement in order to entice existing property owners to improve their properties consistent with the proposed improvements.

Affect on Private Development:

It will be important for the village to carefully negotiate with private property developers regarding the Planning and Design Guidelines. The Planning and Design Guidelines are meant to be flexible in their application and provide for a higher level of quality and aesthetic, while being reasonable to implement. The intent is to create a quality commercial environment that is attractive to developers and benefits the community.

Appendix A
Schematic Streetscape Budget Costs



Preliminary Budget Costs

Date: January 7, 2011
 RE: Cicero Avenue Corridor Plan - 147th Street Streetscape
 Project #: 03-0961-001-01-02

Description	Lamon to 147th-north	Lamon to 147th-south	147th to Kilpatrick-north	147th to Kilpatrick-south	Kilpatrick to Knox-north	Kilpatrick to Knox-south	Knox to Kenton-north	Knox to Kenton-south	Kenton to Kenton-north	Kenton to Kenton-south	Kolmar to Kilbour-north	Kolmar to Kilbour-south	Kilbour to Kilbour-north	Kilbour to Kilbour-south	Unit	Unit Cost	Extended Cost
Site Preparation and Removals																	
remove concrete paving-sidewalk	0	2400	2750	2750	1525	1600	1475	1550	1500	1700	1450	1250	1550	1550	SF	\$3	\$69,150
remove asphalt paving	0	0	0	0	0	0	0	0	0	0	0	0	0	0	SY	\$10	\$0
remove and salvage light pole assembly	3	3	2	3	2	1	1	1	1	2	1	0	1	1	EA	\$500	\$11,000
remove light pole foundation and conduit	3	3	2	3	2	1	1	1	1	2	1	0	1	1	EA	\$500	\$11,000
misc. removals and preparation allowance	2	2	3	3	2	1	1	0	1	2	2	1	1	1	LS	\$1,000	\$22,000
	Subtotal:																\$113,150
Site Grading and Earthwork																	
grading allowance	1	1	2	2	2	1	1	1	1	2	2	1	1	1	LS	\$1,500	\$28,500
	Subtotal:																\$28,500
Storm Drainage and Utilities																	
utility adjustments allowance	2	2	2	2	2	2	2	1	1	2	2	1	1	1	LS	\$500	\$11,500
	Subtotal:																\$11,500
Curbs and Paving																	
concrete curb and gutter (remove and replace)	50	50	50	100	50	50	50	0	50	50	100	100	100	100	LF	\$30	\$27,000
concrete sidewalk paving	4120	4350	3980	4030	2180	2510	2240	2480	2400	2720	2280	2000	2340	2480	SF	\$7	\$280,770
	Subtotal:																\$307,770
Site Furnishings																	
bench	1	1	0	0	0	0	0	0	0	0	0	1	0	0	EA	\$1,500	\$4,500
trash receptacle	1	1	0	0	0	0	0	0	0	0	0	1	0	0	EA	\$1,200	\$3,600
bike rack	3	3	0	0	0	0	0	0	0	0	0	3	0	0	EA	\$500	\$4,500
	Subtotal:																\$12,600
Signage and Amenities																	
regulatory signage w/ decorative pole	1	0	1	1	1	0	1	0	1	1	1	1	1	1	EA	\$1,500	\$16,500
street sign	2	0	2	2	2	0	2	0	2	2	2	2	2	2	EA	\$500	\$11,000
identify sign	2	2	2	2	1	1	1	0	1	1	1	1	1	1	EA	\$5,000	\$85,000
wayfinding sign	1	0	0	0	0	0	0	0	0	1	0	0	0	EA	\$10,000	\$10,000	
gateway feature (secondary)	0	1	0	0	0	0	0	0	0	0	0	0	0	EA	\$15,000	\$15,000	
identify column	0	0	0	0	0	0	0	0	0	0	0	0	0	EA	\$5,000	\$0	
identify wall	0	0	0	0	0	0	0	0	0	0	0	0	0	LF	\$250	\$0	
	Subtotal:																\$137,500

Landscape

parkway tree	12	12	11	11	6	7	5	0	8	8	7	4	6	6	EA	\$600	\$61,800
shrubs (landscape buffer)	127	157	137	105	80	94	80	0	104	114	79	77	82	94	EA	\$50	\$66,500
topsoil	7	9	8	6	5	5	3	0	6	6	5	4	5	5	CY	\$50	\$3,700
hardwood mulch	5	4	3	4	4	4	2	0	5	5	4	3	4	4	CY	\$50	\$2,550
																	Subtotal: \$134,550

Lighting and Electrical

light pole assembly-roadway (incl foundation and allowance for conduit, wiring, controller, service)	3	3	2	3	2	1	1	1	1	2	1	0	1	1	EA	\$15,000	\$330,000
																	Subtotal: \$330,000

Improvements Subtotal: \$1,075,570

General Conditions

traffic control and protection															1	LS	\$21,511	\$21,511
construction layout and staking															1	LS	\$21,511	\$21,511
temporary environmental controls															1	LS	\$10,756	\$10,756
mobilization and contractor general conditions															1	LS	\$32,267	\$32,267
																		Subtotal: \$86,046

Other Project Costs

design and bid contingency (20%)															1	LS	\$215,114	\$215,114
construction contingency (5%)															1	LS	\$53,779	\$53,779
																		Subtotal: \$268,893

Design and Engineering

preliminary/final design and engineering (10%)															1	LS	\$107,557	\$107,557
construction phase services (5%)															1	LS	\$53,779	\$53,779
																		Subtotal: \$161,336

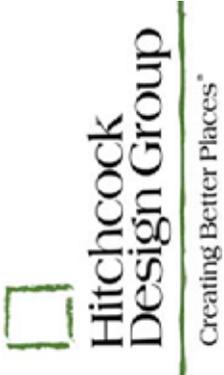
Project Total: \$1,591,844

Square Feet: 66,000

Cost per Square Foot: \$24

Linear Feet: 5,500

Cost per Linear Foot: \$289



Preliminary Budget Costs

Date: January 7, 2011
 RE: Cicero Avenue Corridor Plan - Cicero Avenue Streetscape
 Project #: 03-0961-001-01-02

Description	Limits to 148th-west	Limits to 148th-east	148th to 147th-west	148th to 147th-east	147th to 145th-west	147th to 145th-east	145th to 144th-west	145th to 144th-east	144th to 143rd-west	144th to 143rd-east	Unit	Unit Cost	Extended Cost
Site Preparation and Removals													
remove concrete paving-sidewalk	1500	0	2950	1750	0	780	1575	0	0	650	SF	\$3	\$27,615
remove asphalt paving	0	0	0	0	0	1258	0	0	0	0	SY	\$10	\$12,580
remove and salvage light pole assembly	2	2	4	4	9	10	5	5	3	2	EA	\$500	\$23,000
remove light pole foundation and conduit	2	2	4	4	9	10	5	5	3	2	EA	\$500	\$23,000
misc. removals and preparation allowance	1	1	2	3	5	5	3	3	2	2	LS	\$1,000	\$25,000
												Subtotal:	\$111,195
Site Grading and Earthwork													
grading allowance	1	1	2	3	5	5	2	2	1	1	LS	\$1,500	\$33,000
												Subtotal:	\$33,000
Storm Drainage and Utilities													
utility adjustments allowance	1	1	2	2	5	5	3	3	2	2	LS	\$500	\$12,000
												Subtotal:	\$12,000
Curbs and Paving													
concrete curb and gutter (remove and replace)	50	25	50	100	100	100	50	50	50	50	LF	\$30	\$17,250
concrete sidewalk paving	2367	2052	5868	5242	12222	11914	0	5408	2280	0	SF	\$7	\$331,471
stamped concrete paving (carriage walk)	340	260	1060	700	2700	2250	1220	1130	400	0	SF	\$12	\$120,720
												Subtotal:	\$469,441
Site Furnishings													
bench	0	0	2	1	1	1	0	0	0	0	EA	\$1,500	\$7,500
trash receptacle	0	0	2	1	1	1	0	0	0	0	EA	\$1,200	\$6,000
bike rack	0	0	3	0	0	0	0	0	0	0	EA	\$500	\$1,500
planter pot	0	0	3	3	3	3	0	0	0	0	EA	\$2,500	\$30,000
tree grate	4	2	11	9	29	25	13	11	4	5	EA	\$2,500	\$282,500
												Subtotal:	\$327,500

Signage and Amenities

regulatory signage w/ decorative pole	1	0	0	0	0	1	1	1	1	1	EA	\$1,500	\$9,000
street sign	2	0	2	2	2	2	2	2	2	2	EA	\$500	\$9,000
identity sign	1	0	2	2	4	4	2	1	1	1	EA	\$5,000	\$105,000
wayfinding sign	0	0	1	0	0	0	0	0	0	0	EA	\$5,000	\$10,000
gateway feature	0	1	0	0	0	1	0	0	0	0	EA	\$25,000	\$50,000
identity column	0	0	2	2	0	0	0	0	0	0	EA	\$5,000	\$30,000
identity wall	0	0	60	50	100	0	0	0	0	0	LF	\$250	\$52,500
Subtotal:												\$265,500	

Landscape

parkway tree	4	4	11	9	29	25	13	11	4	5	EA	\$600	\$69,000
shrubs (landscape buffer)	65	60	75	140	435	350	205	170	75	80	EA	\$50	\$82,750
topsoil	15	15	25	20	20	20	12	10	4	5	CY	\$50	\$7,300
hardwood mulch	7	7	10	10	25	25	10	10	3	3	CY	\$50	\$5,500
Subtotal:												\$164,550	

Lighting and Electrical

light pole assembly-roadway (incl foundation and allowance for conduit, wiring, controller, service)	2	2	4	4	9	10	5	5	3	2	EA	\$15,000	\$690,000
Subtotal:												\$690,000	

Improvements Subtotal: \$2,073,186

General Conditions

traffic control and protection										1	LS	\$41,464	\$41,464
construction layout and staking										1	LS	\$41,464	\$41,464
temporary environmental controls										1	LS	\$20,732	\$20,732
mobilization and contractor general conditions										1	LS	\$62,196	\$62,196
Subtotal:												\$165,855	

Other Project Costs

design and bid contingency (20%)										1	LS	\$414,637	\$414,637
construction contingency (5%)										1	LS	\$103,659	\$103,659
Subtotal:												\$518,297	

Design and Engineering

preliminary/final design and engineering (10%)										1	LS	\$207,319	\$207,319
construction phase services (5%)										1	LS	\$103,659	\$103,659
Subtotal:												\$310,978	

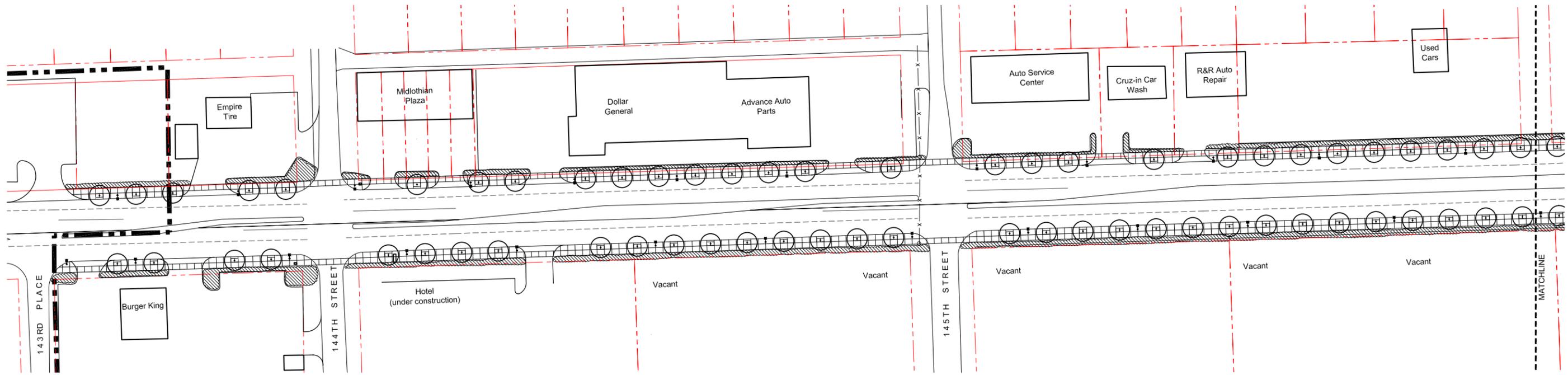
Project Total: \$3,068,315

Square Feet: 74,400
Cost per Square Foot: \$41

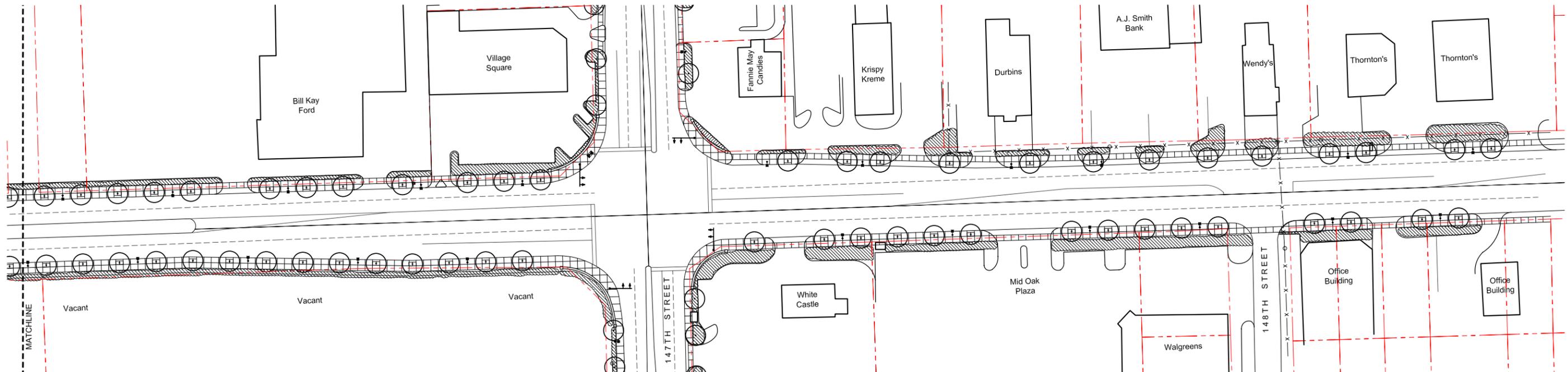
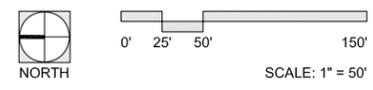
Linear Feet: 6,200
Cost per Linear Foot: \$495

Appendix B

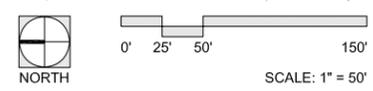
Schematic Streetscape Plan - Cicero Avenue and 147th Street



1 Cicero Avenue from 143rd Place to 146th Street
Plan



2 Cicero Avenue from 146th Street to 148th Street
Plan



PREPARED FOR:
Village of Midlothian

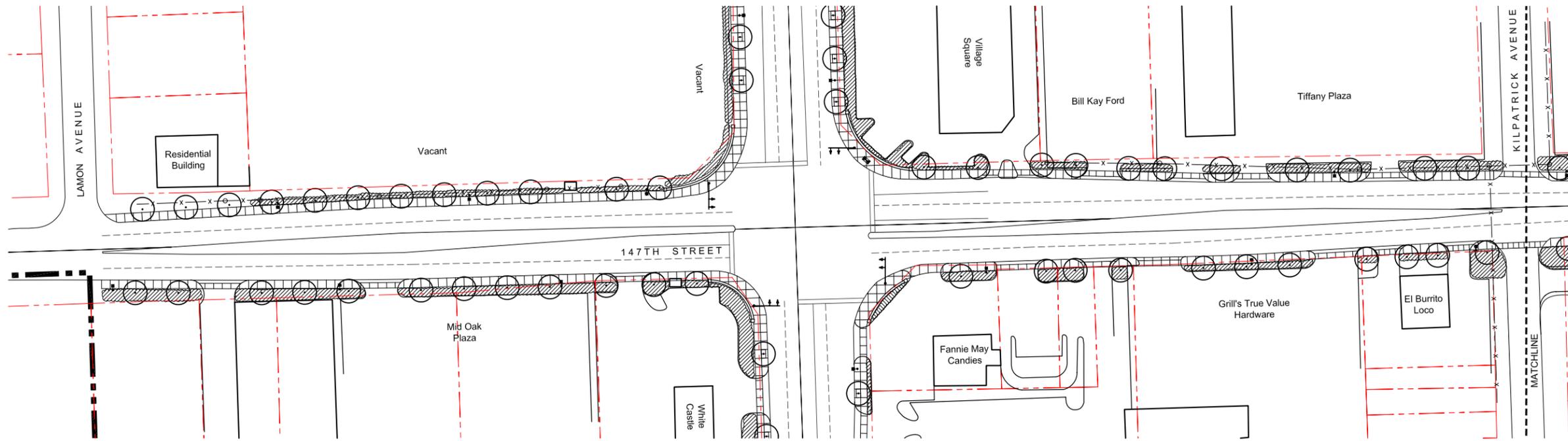
Cicero Avenue

Cicero Avenue Corridor Plan

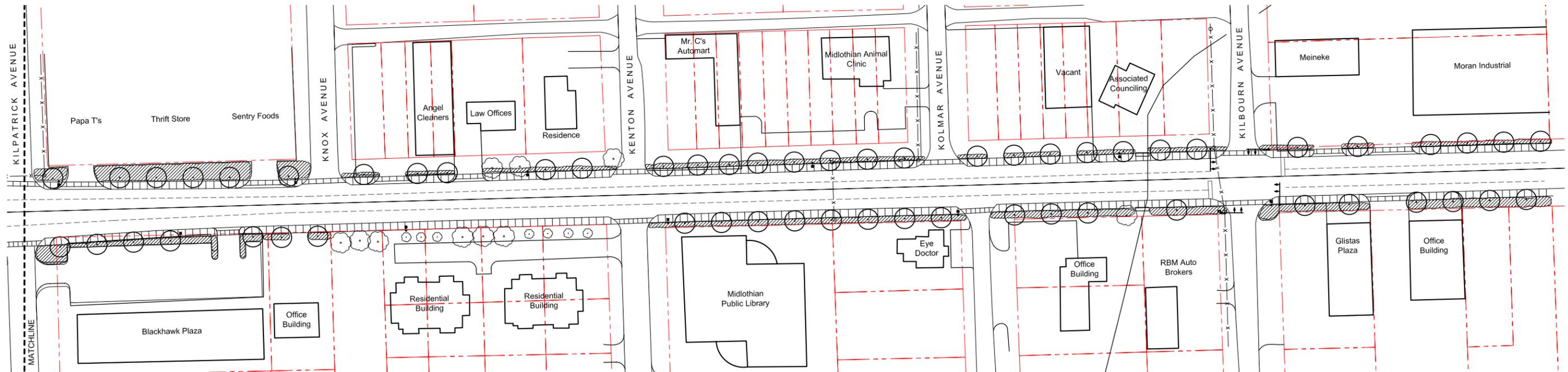
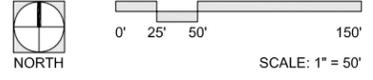
Midlothian, Illinois



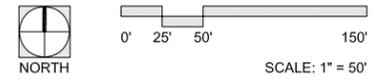
ISSUE DATE: December 14, 2010
PROJECT NUMBER: 03-0961-001-01-03
All drawings are preliminary and subject to change.
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1 147th Street from Lamon Avenue to Kilpatrick Avenue
Plan



2 147th Street from Kilpatrick Avenue to Kilbourn Avenue
Plan



PREPARED FOR:
Village of Midlothian

147th Street

Cicero Avenue Corridor Plan

Midlothian, Illinois

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ISSUE DATE: December 14, 2010
PROJECT NUMBER: 03-0961-001-01-03
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